

**SAS Superstructure**

Location: 04-SF-80-13.2 / 13.9

Client Name: CalTrans

Run date 20-Nov-14

Time 9:30 AM

**Daily Diary Report by Bid Item**

Contract No.: 04-0120F4

Diary #: 042 Const Calendar Day: 244 Date: 10-May-2010 Monday

Inspector Name: Iranmanesh, Abbas Title: Transportation Engineer

Inspection Type:

Shift Hours: Break: Over Time:

Federal ID:

Location:

Reviewer: Liu, Tai-Lin Approved Date: 08-Jul-10 Status: Approved

**04-0120F4  
04-SF-80-13.2/13.9  
Self-Anchored  
Suspension Bridge****Weather**

Temperature 7 AM 12 PM 4PM

Precipitation Condition

Working Day ☐ If no, explain:**Diary:**

Dispute

**Work description.**

Field Work at Pier 7



Lift 5W:

- 1- Two ABF crews were resurfacing the OBG skin plate and non-bolted connections, one at the splice 4 (4W to 5W) and the other at the splice 5 (5W to 6W).
- 2- Two ABF crews were drilling holes in order to put in place the plate connecting the OBG to the cradle for the OBG lateral support purpose.
- 3- Two OBG crews were involving in transferring the ventilation ducts and other necessary items inside the OBG.
- 4- Crane RT160 and its operator were involved in transferring the material either inside the OBG or on the deck.
- 5- A bundle of 4 1/2"x3"x15' woods was transferred on the deck.
- 6- Three counterweight boxes and four suspender brackets were put in place on Friday May 7, 2010.

Equipments: Man lift ID # 481-93-6009  
Man lift ID # 477-40-5024  
Power Generator: Whisperwatt 700 (549-07-9004)  
Crane 160T  
Lift truck 76706

Field Work at Pier W2

Items # 838 &amp; 839

- 1- Item # 838: It was noted that at the faying surfaces of the splice plate to the deck plate of the crossbeam connecting 3AW to CB2 there is no gap and the surfaces at both sides, the decks and the crossbeam match each other very well.
- 2- Item # 839: It was noted that at the faying surfaces of the splice plate to the deck plate of the crossbeam connecting 3AE to CB2 there is no gap and the surfaces at both sides, the decks and the crossbeam match each other very well.
- 3- To show the quality of the crossbeams CB1, CB2, and CB3 connections to the W-Line and E-Line deck a few pictures are attached. There was no access to check the connection below the deck.
- 4- The permanent bolts of the following suspender brackets at the specified panel points have been done in China:
  - W-Line deck: PP14, PP20, PP22, PP38, PP40, PP42, PP44 and PP46
  - E-Line deck: PP42.



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. Total # of M24 HS Bolts required to replace Type1 Suspender brackets at PP14, and PP20:

$3 \times 270 = 810 \text{ HSB}$

. Total # of M24 HS Bolts required to replace Type2 Suspender brackets at PP38, PP40, 2xPP42, PP44, and PP46:  $6 \times 294 = 1764 \text{ HSB}$

Total M24 Bolts required =  $810 + 1764 = 2574 \text{ HSB}$

NOTE: In the inspection process Victor has been present.

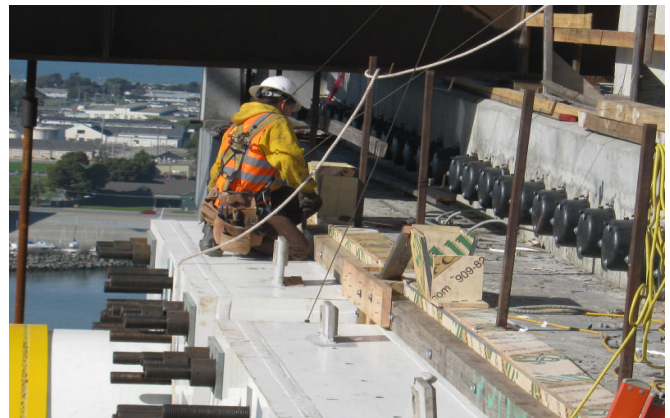
### Office Work

1- Reviewing punch list of the OBG lifts 1 through 4.

2- Working on the daily report.

Hours worked: 9 Hours, 1 hour OT.

### Attachment



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